

STATE	SECTION	SHEET	TOTAL
U	ARK.	15301	1
100	NO	150	26

ARKANSAS STATE HIGHWAY COMMISSION

DWIGHT H. BLACKWOOD

CHAIRMAN

JUSTIN MATTHEWS J. LAN WILLIAMS

J. S. PARKS SAM J. WILSON

COMMISSIONERS

HIGHWAY BRIDGE OVER WHITE RIVER
AT

CLARENDON, ARKANSAS

(MONROE COUNTY)

C. S. CHRISTIAN

HIGHWAY ENGINEER

JOB NO. 199

IRA G. HEDRICK, INC.

N. B. GARVER

BRIDGE ENGINEER

U.S.R. 79 S-13&14

CONSULTING ENGINEERS

INDEX

HOT SPRINGS, ARKANSAS

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TITLE SHEET
GENERAL PLAN AND PROFILE (ORIGINAL)
GENERAL PLAN AND PROFILE (REVISED)
PROFILE OF EAST OLD RIVER & WHITE RIVER CROSSINGS
PROFILE OF ROCK RIVER BAYOU & WEST OLD RIVER CROSSINGS
STRESSES & SECTIONS
DETAILS SUSPENDED SPAN
DETAILS CANTILEVER ARMS
DETAILS OF ANCHOR ARM
DETAILS OF APPROACH GIRDER SPANS, HAND RAILS & LAMP POSTS
SUPPLEMENTAL DETAILS FOR MAIN BRIDGE

DETAILS OF EAST APPROACH
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DETAILS OF EAST APPROACH
DETAILS OF WEST APPROACH
DETAILS OF TOLL TAKER'S RESIDENCE
DETAILS OF TOLL TAKER'S RESIDENCE

GENERAL NOTES

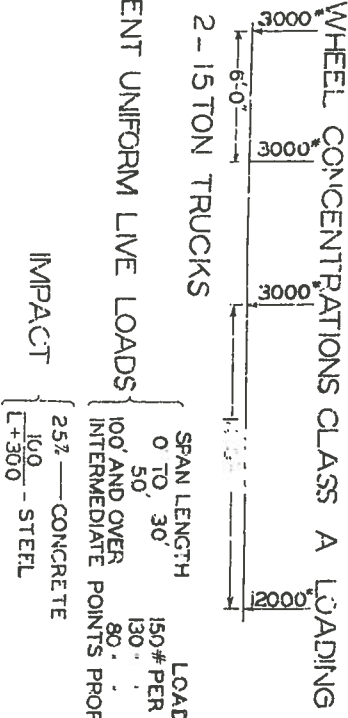
ALL HOOPS ON REINFORCING BARS TO HAVE A RADIUS OF 40 AND A RETURN OF 120 WHERE D-DIAMETER OF ROUND BAR OR SIDE OF SQUARE BAR. LENGTHS OF HOOPED BARS ARE GIVEN TO STARTING POINT OF HOOK, PLUS: 1. ALL BENDS SUCH AS ON GIRDER BARS MUST HAVE A RADIUS OF NOT LESS THAN 120.
CENTERS OF BARS IN FLOOR SLABS TO BE NOT LESS THAN 1/2" FROM FACE OF CONCRETE. CENTERS OF BARS TO BE 4" FROM FACE OF CONCRETE ON BEAMS AND ADJUTANT SHAFTS AND 3" ON GIRDERS AND COLUMNS UNLESS OTHERWISE NOTED.
CHANGERS TO BE 2" THROUGHOUT UNLESS OTHERWISE NOTED.
AT ALL SPLICES, BARS TO BE LAPPED NOT LESS THAN 40D.

CONSTANTS

FLOOR SLABS, CROSS GIRDERS, GIRDERS, ETC.
BEAMS CONTINUOUS OVER SUPPORTS: AT CENTER OF BEAMS 975 # PER SQ. IN.
BOND FOR STEEL IN CONCRETE 11.25 "
COLUMNS IN DIRECT COMPRESSION 150 "
NOTE FOR 1-2-4 CONCRETE DECREASE ABOVE STRESSES 3%
TENSION NET SECTION 300-42.5 L.
COMP. IN COLS. & OTHER COMP. MEMBERS, FIXED ENDS 900-42.5 L.
MODULUS OF ELASTICITY, STEEL 30000000 "
VARIATION IN TEMPERATURE 1-2-4 3000000 "
COEFFICIENT OF EXPANSION 1-2-4 1.50"
0000060 0000067

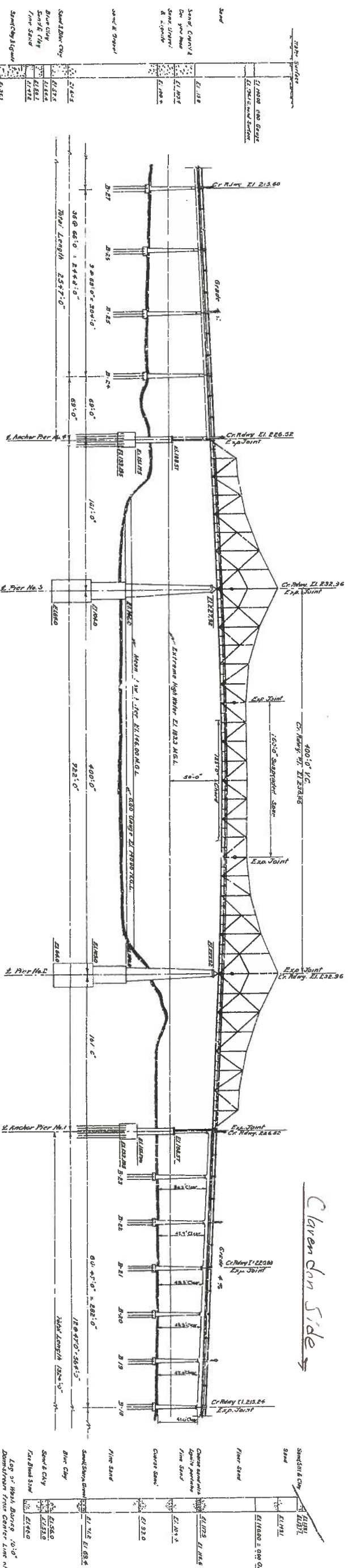
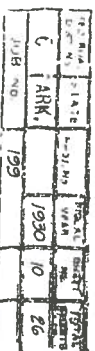
Be. No. 1253

Draw. No. 4901



IMPACT

SPAN LENGTH
0 TO 30' 150# PER SQ. FT.
50' 130#
80' 130#
100' AND OVER 130#
INTERMEDIATE POINTS PROPORTIONAL
25% CONCRETE
100% STEEL
L+300



NOTE: Elevations of bottoms of column, as follows: 1 to 23 in.; Bent 34 8.75; Bent 26 11.52-30; Bents 27 to 33 in.; Bent 34 16.40; Bents 35 to 39 in.; Bent 40 17.55-30; Bents 41 to 50 in.; 11.16-40.

This technical drawing illustrates a long bridge structure composed of several spans. Key features include:

- Span Annotations:** Multiple spans are labeled with "Exp. E.I." values: 215.76, 203.95, 189.18, 179.36, 189.56, and 184.76.
- Street Crossings:** Two streets cross under the bridge: "Red Street" near the top right and "S. Street" further down.
- Dimensions:** Vertical dimensions on the left indicate heights or clearances: 122'-0", 106'-0", 150'-0", and 120'-0".
- Structural Details:** The drawing shows the profile of the bridge deck, support piers, and the internal structural members (beams and girders) with rivet patterns.
- Termination:** The structure ends at the bottom right with the label "End Structure E.I. 173.36".
- Orientation:** A vertical arrow on the far right points upwards and is labeled "Clearance".

MAIN RIVER AND EAST GL. CROSSINGS

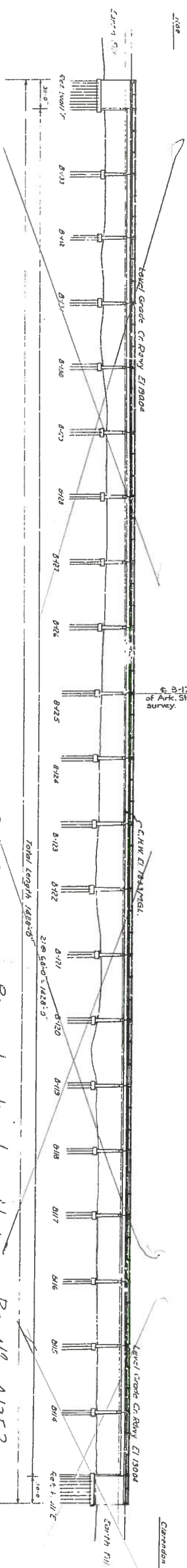
Bowling East Approach Fly & Fly: 12/11/18 Column - Sept. 1, 30

ARKANSAS STATE HIGHWAY COMMISSION
BRIDGE OVER WHITE RIVER
CLARENDON, ARKANSAS.
PROFILE OF EAST OLD RIVER
& WHITE RIVER CROSSINGS
KDOT E 10 79 SEC 15
MADE BY WCH
TRACED BY WCH
CHECKED BY ILD
DATE: APRIL 25, 1990 SCALE: 1"=50'-0"
SHEET NO. 10
R.A.G. HEDRICK, INC.
CONSULTING ENGINEERS
HOT SPRINGS ARKANSAS
Dwg. No. 4903

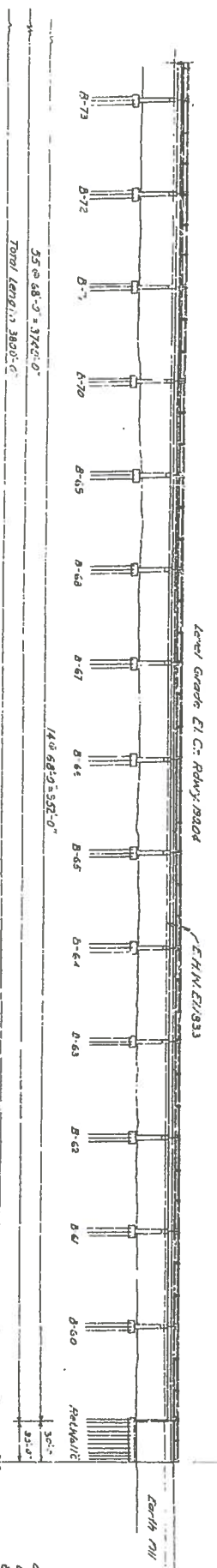
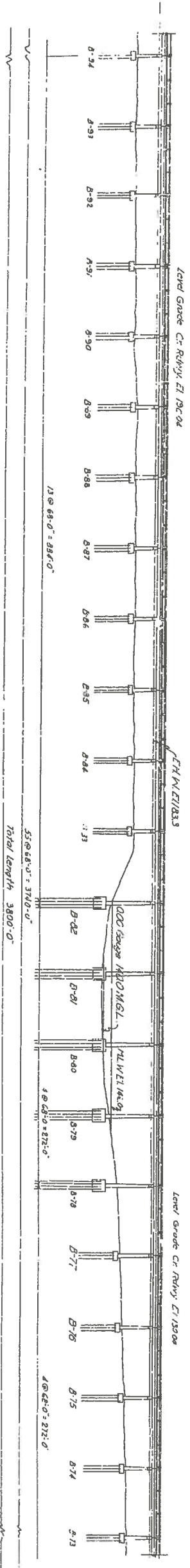
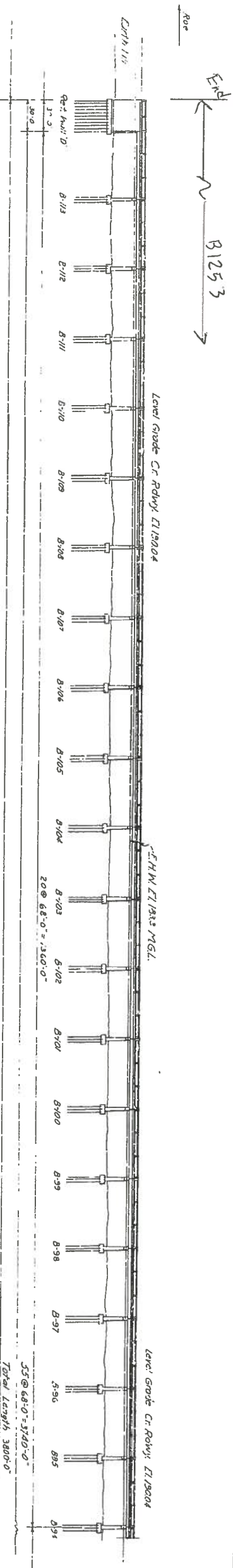
BR, N^o 01253 (4 piers,

DATE	BY	NO.	REV.
1930	II	26	

333



Removed already. It was Ret. W. E. F., plus Bt's 114 to 133.



Notes: Elevation bottom of column:
Bents 60 to 74 incl. El. 154.5
Bents 75 to 77 incl. El. 156.0
Bents 78 to 82 incl. El. 142.0
Bents 83 to 88 incl. El. 164.5
Bents 89 to 95 incl. El. 162.0
Bents 96 to 105 incl. El. 162.0
Bents 106 to 134 incl. El. 162.0

The bottom of the remaining footings and retaining walls C, D, E, & F, to be placed so that they will be at least 6'-0" below the surface of the ground. Excavate just out of all water and retaining walls on West Approach.

Revised West Old River Crossing, Aug. 20, 33
Revised, July 10, 30, K.E.

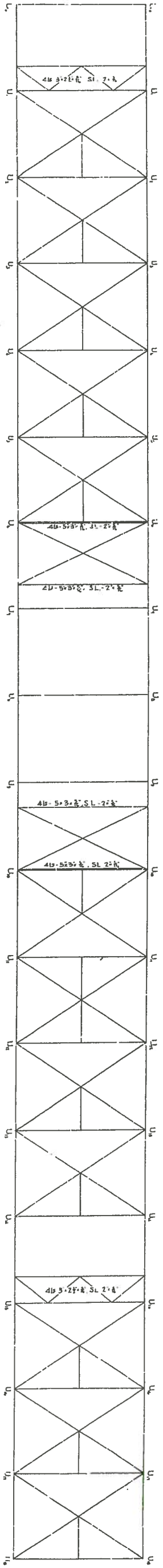
MADE BY W.C.H.
TRACED BY L.C.H.
CHECKED BY W.C.H.
DATE APR. 28, 1930 SCALE 1" = 50'-0"
SHEET NO. 11

WEST OLD RIVER CROSSING (BR. No. B1253)
CONSISTS OF RETAINING WALLS AND
AND BT. Nos. 60 to 113.

BR. No. B1253

DEMS. No. 4904

1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	19	20	21	22	23	24	25	26	27	28	29	30	31	32	33	34	35	36	37	38	39	40	41	42	43	44	45	46	47	48	49	50	51	52	53	54	55	56	57	58	59	60	61	62	63	64	65	66	67	68	69	70	71	72	73	74	75	76	77	78	79	80	81	82	83	84	85	86	87	88	89	90	91	92	93	94	95	96	97	98	99	100
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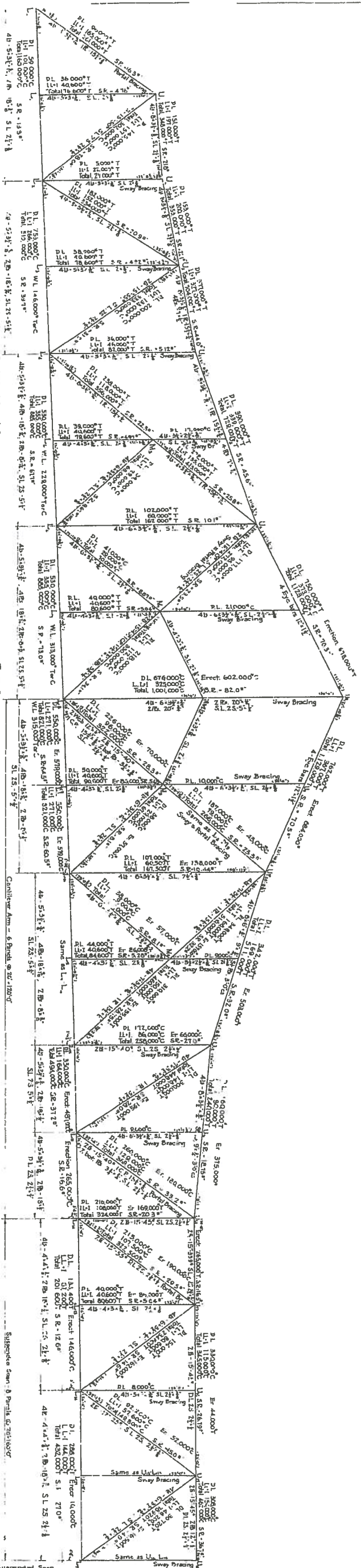


All Diagonal and Transverse Struts - 48-55 3/4" x 5/8" SL-25 K
All Longitudinal Struts - 20-3x21 K

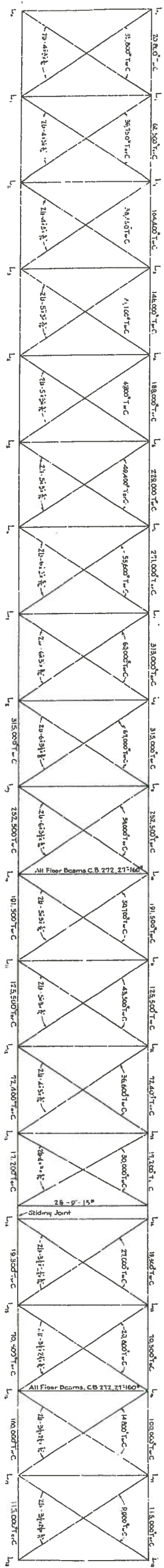
UPPER LATERAL SYSTEM

All Diagonal and Transverse Struts - 48-55 3/4" x 5/8" SL-25 K
All Longitudinal Struts - 20-3x21 K

All Diagonal and Transverse Struts - 48-55 3/4" x 5/8" SL-25 K
All Longitudinal Struts - 20-3x21 K



TRUSS SYSTEM



LOWER LATERAL SYSTEM

All Lower Lateral Diagonal Struts - 48-55 3/4" x 5/8" SL-25 K
All Struts - 20-3x21 K

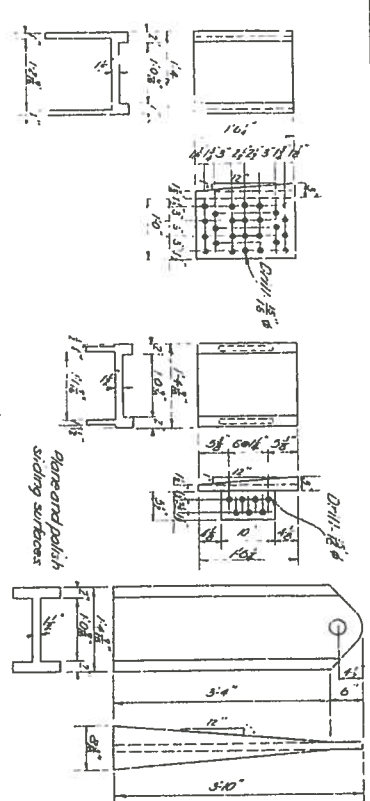
ARKANSAS STATE HIGHWAY COMMISSION BRIDGE OVER WHITE RIVER CLARENDON, ARKANSAS.

STRESSES & SECTIONS

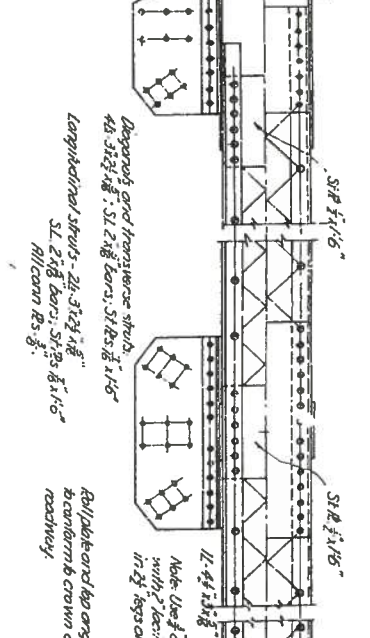
MADE BY: R.F.
CHECKED BY: R.F.
DATE: April 15, 1935
SCALE: 1/4" = 1'-0"

DESIGNED BY: R.F.
DATE: April 15, 1935
SCALE: 1/4" = 1'-0"

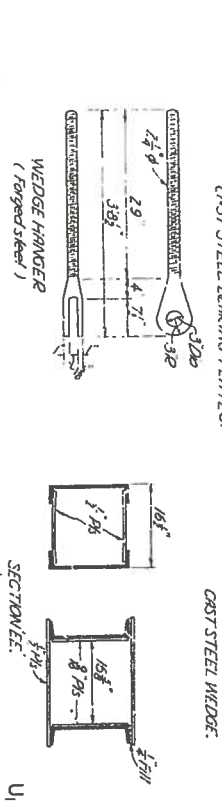
Note:
Each Iron Diagonal is to be connected at its ends to the following panel points: L1, L2, L3, L4, L5, L6, L7, L8, L9, L10, L11, L12, L13, L14, L15, L16, L17, L18, L19, L20, L21, L22, L23, L24, L25, L26, L27, L28, L29, L30, L31, L32, L33, L34, L35, L36, L37, L38, L39, L40, L41, L42, L43, L44, L45, L46, L47, L48, L49, L50, L51, L52, L53, L54, L55, L56, L57, L58, L59, L60, L61, L62, L63, L64, L65, L66, L67, L68, L69, L70, L71, L72, L73, L74, L75, L76, L77, L78, L79, L80, L81, L82, L83, L84, L85, L86, L87, L88, L89, L90, L91, L92, L93, L94, L95, L96, L97, L98, L99, L100, L101, L102, L103, L104, L105, L106, L107, L108, L109, L110, L111, L112, L113, L114, L115, L116, L117, L118, L119, L120, L121, L122, L123, L124, L125, L126, L127, L128, L129, L130, L131, L132, L133, L134, L135, L136, L137, L138, L139, L140, L141, L142, L143, L144, L145, L146, L147, L148, L149, L150, L151, L152, L153, L154, L155, L156, L157, L158, L159, L160, L161, L162, L163, L164, L165, L166, L167, L168, L169, L170, L171, L172, L173, L174, L175, L176, L177, L178, L179, L180, L181, L182, L183, L184, L185, L186, L187, L188, L189, L190, L191, L192, L193, L194, L195, L196, L197, L198, L199, L200, L201, L202, L203, L204, L205, L206, L207, L208, L209, L210, L211, L212, L213, L214, L215, L216, L217, L218, L219, L220, L221, L222, L223, L224, L225, L226, L227, L228, L229, L230, L231, L232, L233, L234, L235, L236, L237, L238, L239, L240, L241, L242, L243, L244, L245, L246, L247, L248, L249, L250, L251, L252, L253, L254, L255, L256, L257, L258, L259, L260, L261, L262, L263, L264, L265, L266, L267, L268, L269, L270, L271, L272, L273, L274, L275, L276, L277, L278, L279, L280, L281, L282, L283, L284, L285, L286, L287, L288, L289, L290, L291, L292, L293, L294, L295, L296, L297, L298, L299, L300, L301, L302, L303, L304, L305, L306, L307, L308, L309, L310, L311, L312, L313, L314, L315, L316, L317, L318, L319, L320, L321, L322, L323, L324, L325, L326, L327, L328, L329, L330, L331, L332, L333, L334, L335, L336, L337, 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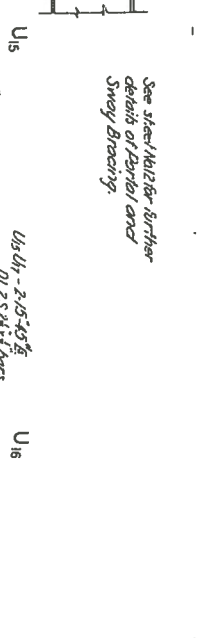
NOTE: Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span.



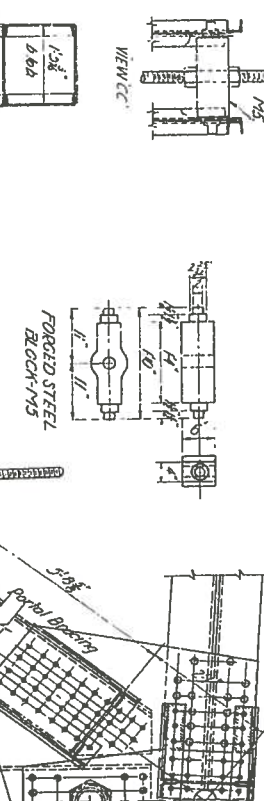
NOTE: This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span.



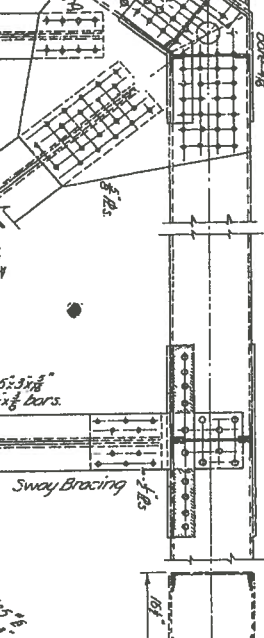
NOTE: Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span.



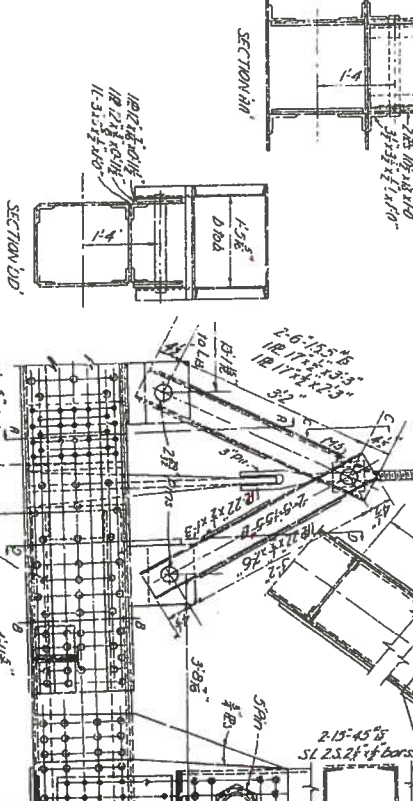
NOTE: This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span.



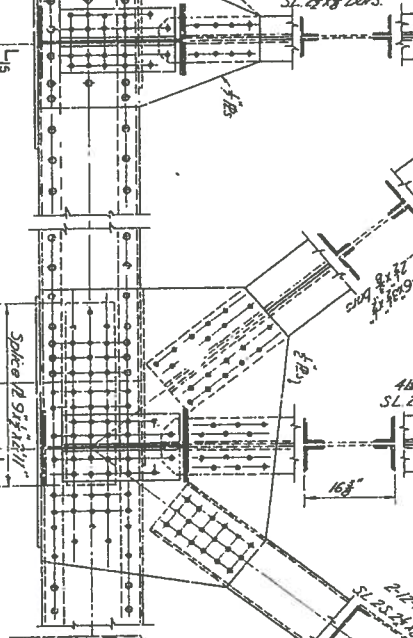
NOTE: Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span.



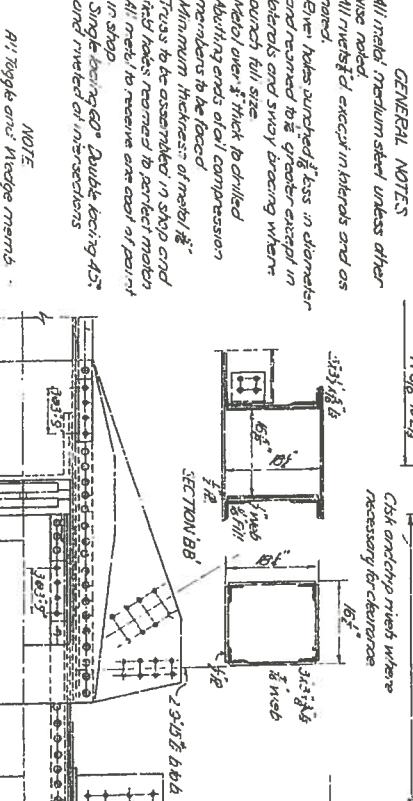
NOTE: This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span.



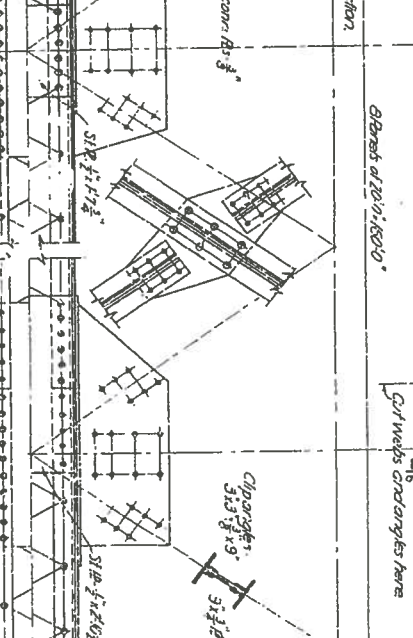
NOTE: Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span.



NOTE: This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span.



NOTE: Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span. Wedges to be removed after erection and of bearing of suspended span.



NOTE: This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span. This expansion bracket is to be removed after erection and of bearing of suspended span.

REVISIONS:

- REVISED JUNE 12, 1930
- ARKANSAS STATE HIGHWAY COMMISSION
- BRIDGE OVER WHITE RIVER
- CLARENCE, ARKANSAS

DETAILS SUSPENDED SPAN

MADE BY: J. S. HEDRICK, INC.

SCALE: 1/4" = 1'-0"

DATE: APRIL 25, 1930

REVISIONS:

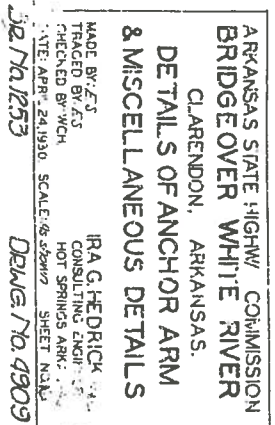


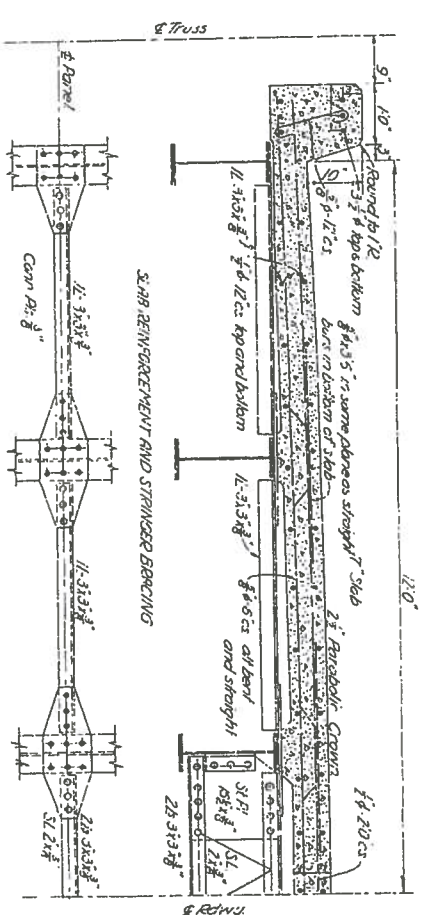
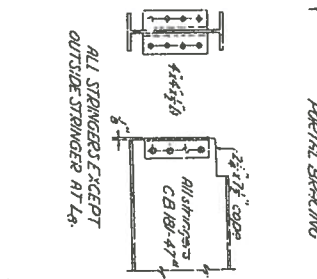
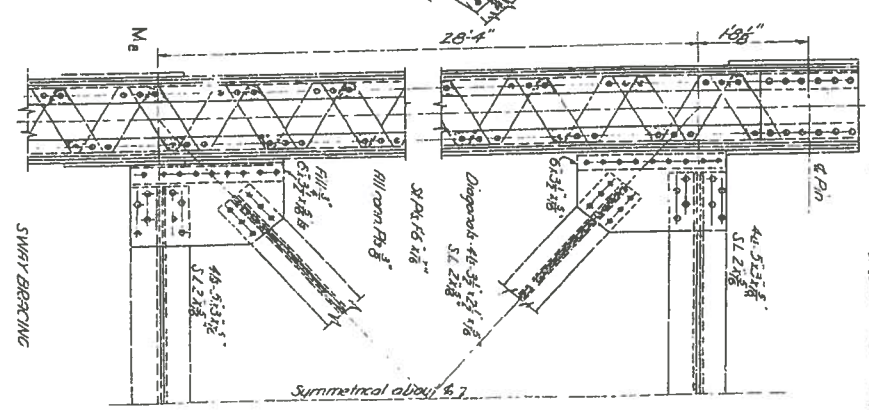
DETAILS CANTILEVER AFF

DATE: APRIL 24, 1930 SCALE AS SHOWN SHEET NO. 14
 TRACED BY WCH & LS CONSULTING ENGINEERS
 CHECKED BY WCH HOT SPRINGS, ARKANSAS

BR. No. 1253 DRWG. No. 4907







Scale. All dimensions in feet.

All metal members shall unless otherwise noted, all $\frac{1}{2}$ " gage except in brackets and as noted. Fast holes to be punched $\frac{1}{16}$ " less in diameter and reamed to the greater except in brackets and such bracket where such full size metal over $\frac{1}{2}$ " thick to be drilled $\frac{1}{16}$ ". Minimum thickness of metal $\frac{1}{8}$ ".

Flange ends of all compression members to be faced.

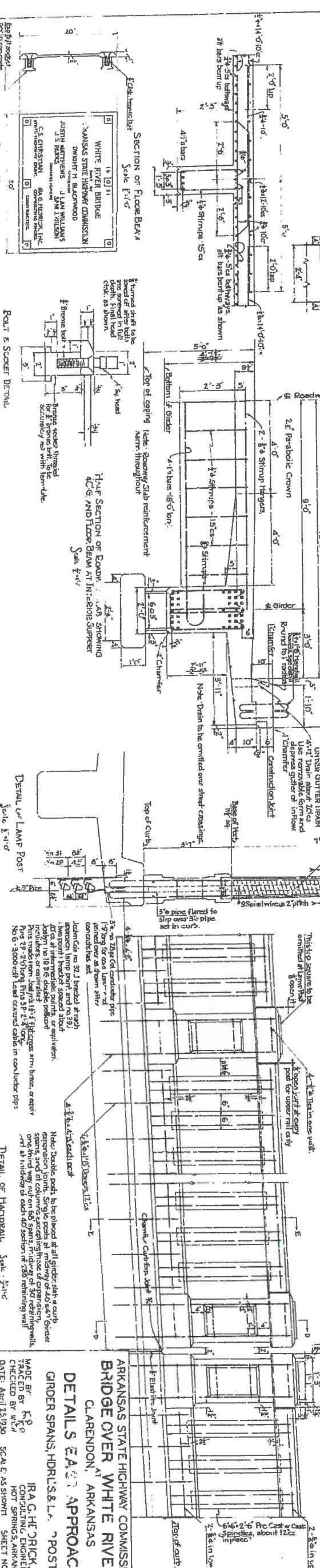
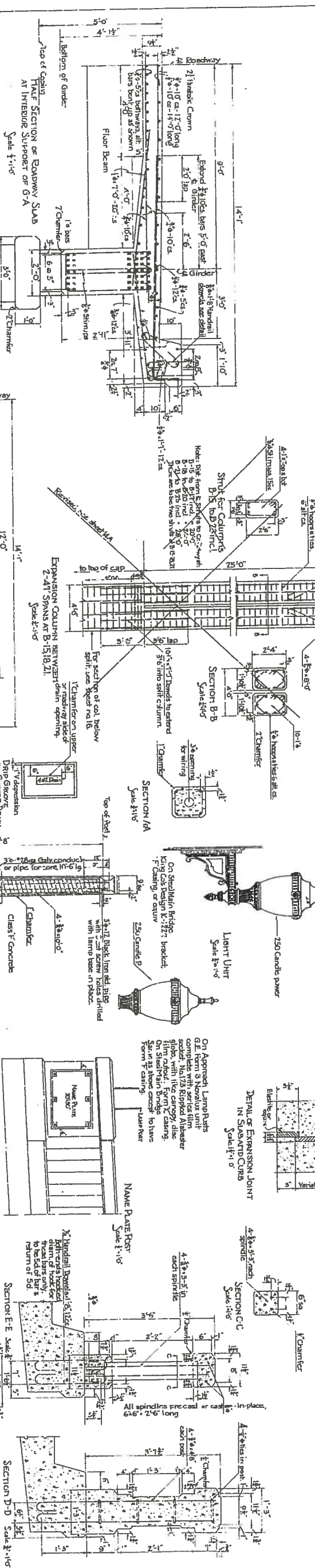
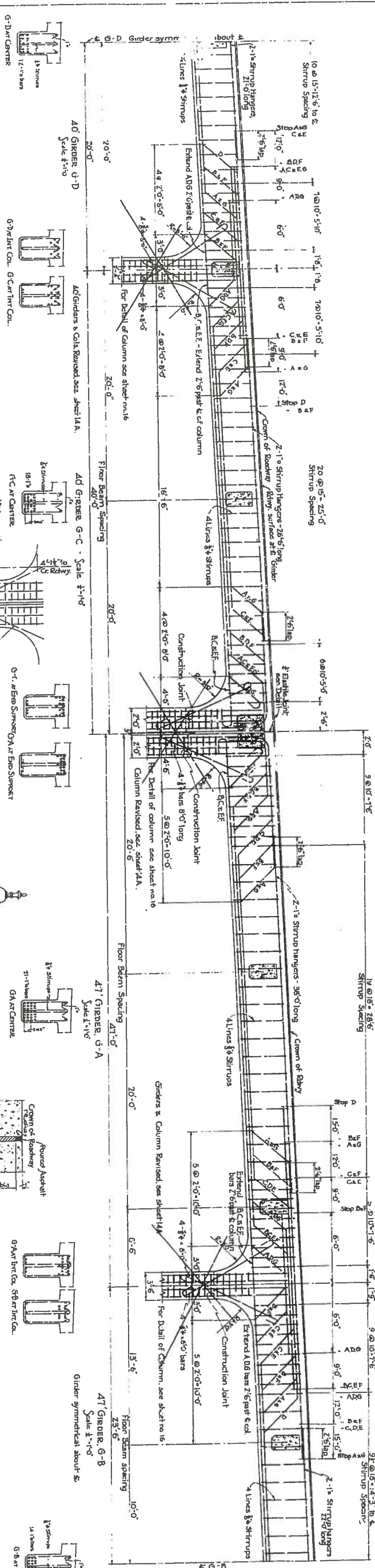
Truss to be assembled in shop and field holes reamed to fit. 1" metal plates reamed to fit. 1" metal plates to receive 1" steel of bent in shop single lacing. 6" double lacing 45° and riveted in field sections.

ARKANSAS STATE HIGHWAY COMMISSION
BRIDGE OVER WHITE RIVER
CLARENDON, ARKANSAS
SUPPLEMENTAL DETAILS
STEEL SPANS

MADE BY: E.S. & L.
TRACED BY: E.S.
CHECKED BY: WJCH
DATE: APRIL 25, 1930
SCALE: As shown
SHEET: NO. 17

IRAC. HEDRICK INC
CONSULTING ENGINEERS,
HOT SPRINGS, ARKANSAS.

De. No. 1253
Drawg. No. 4910

341

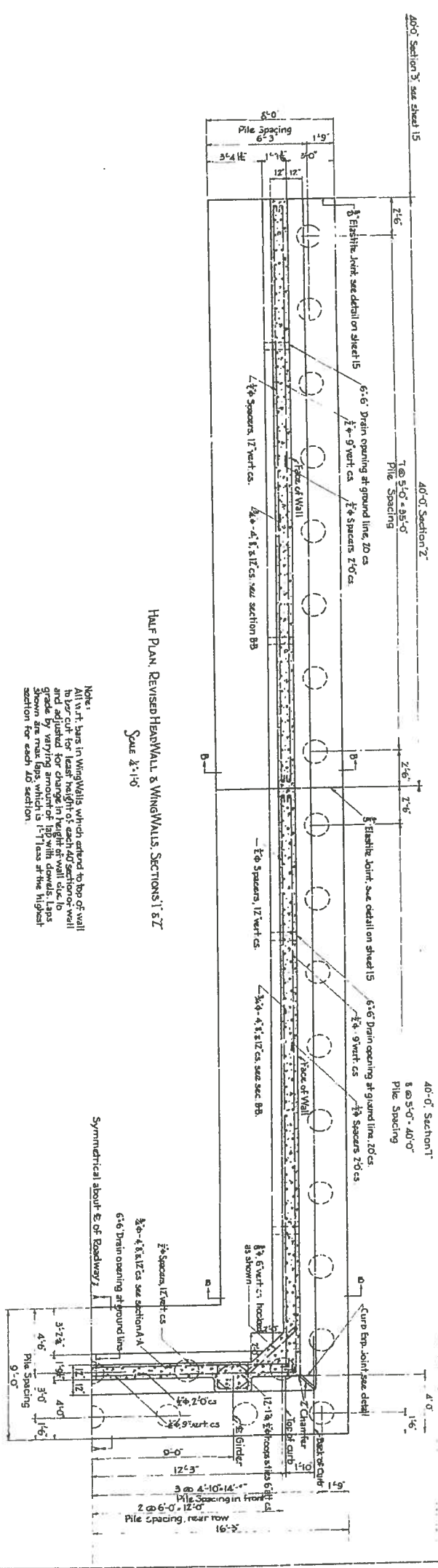
girder, slab a curb
 (width of 40 sq ft) under
 use of expansion,
 1/2 of 50 retaining walls,
 150 retaining wall
 150 retaining wall
 150 retaining wall

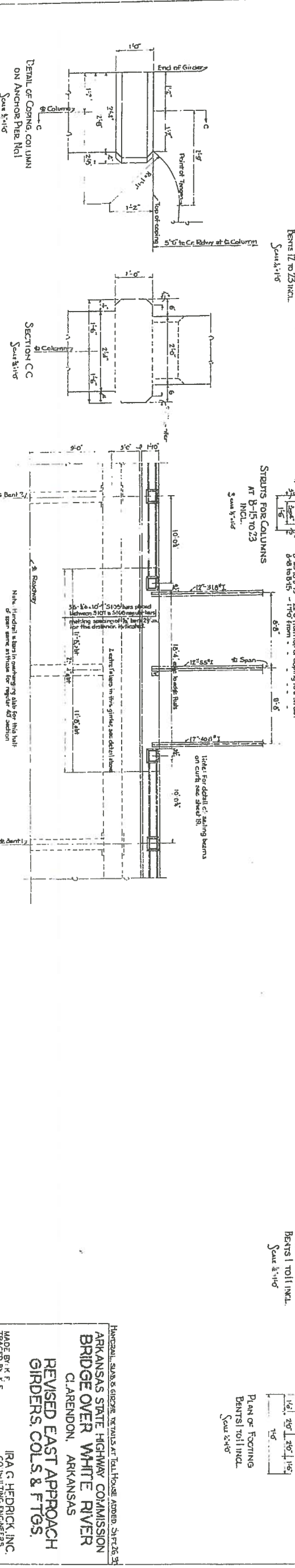
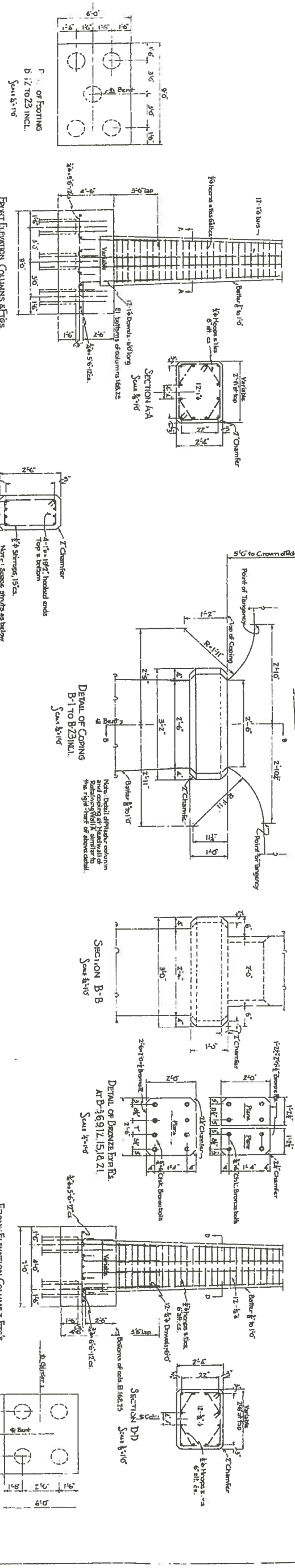
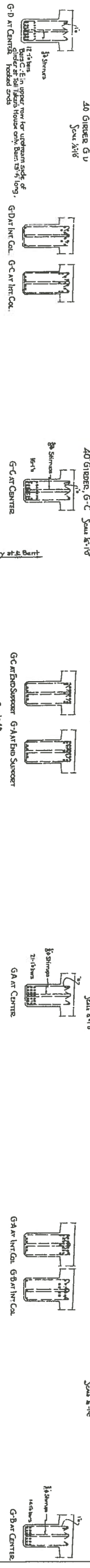
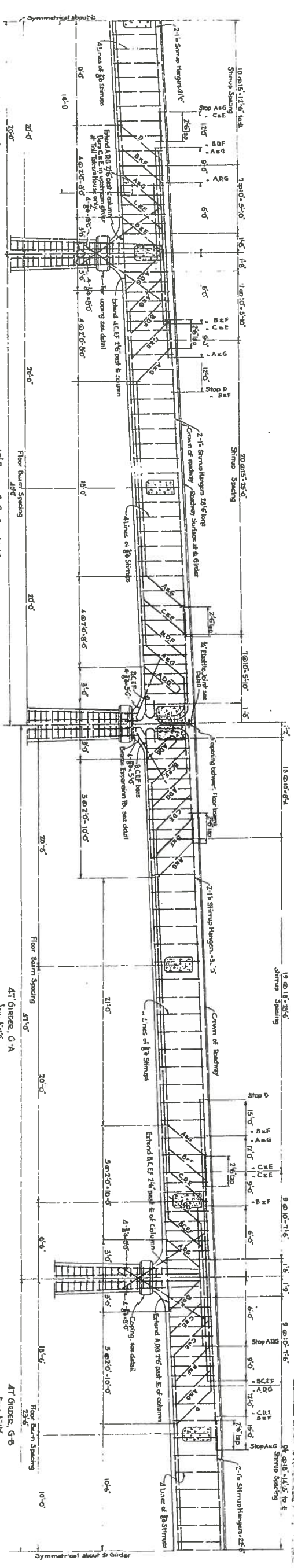
Be No. 1253

ARKANSAS STATE HIGHWAY COMMISSION
 BRIDGE OVER WHITE RIVER
 CLARENDON, ARKANSAS
 DETAILS E.A. APPROACH
 GIRDER SPANS, HDRLS & L. POSTS
 MADE BY K.O.
 TRACED BY K.O.
 CHECKED BY W.E.H.
 DATE: JUNE 19, 1930
 SCALE AS SHOWN
 SHEET NO. 19
 IRAC, HE, CRICK, INC.
 CONSULTING ENGINEERS
 HOT SPRINGS, ARKANSAS

REVISED: JUNE 19, 1930
 REVISION: GIRDER, HDRLS, SHEET NO. 19

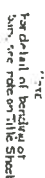
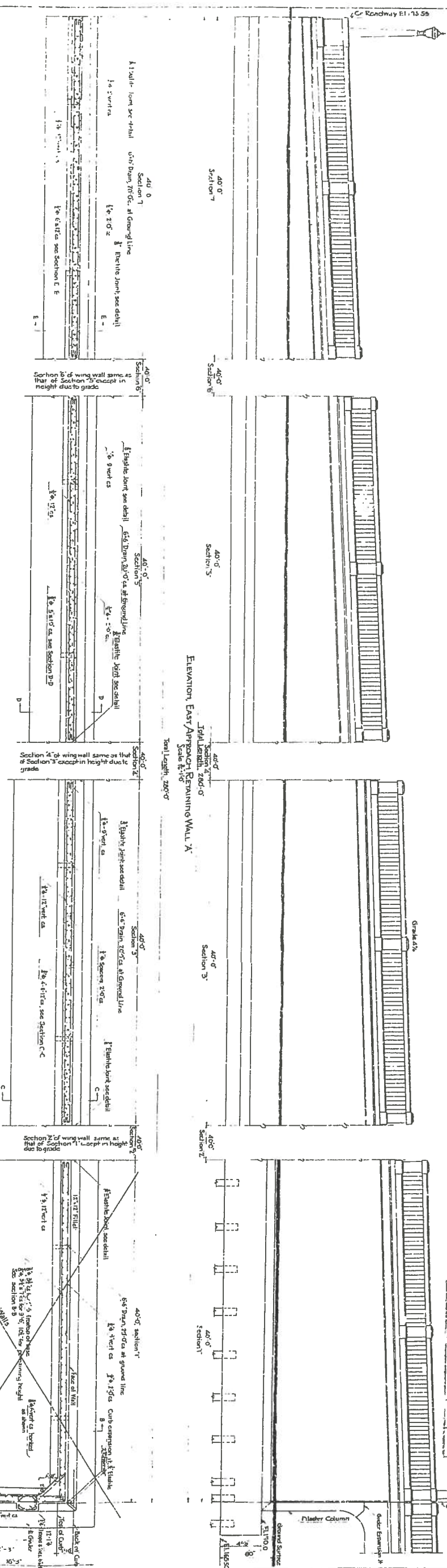
Be No. 1253





MADE BY: K. F. HEDRICK, INC.
CHECKED BY: W. C. HEDRICK, INC.
DATE: SEPT. 1930. SCALE AS SHOWN. SHEET NO. 20
BRIDGE NO. 1253
DRAWING NO. 4912-A

NAME	DATE	POST NO.	VLAN	SPLIT NO.	TOTAL BUDGET
B. BIKI,			1930	21	26
JANUARY		199			

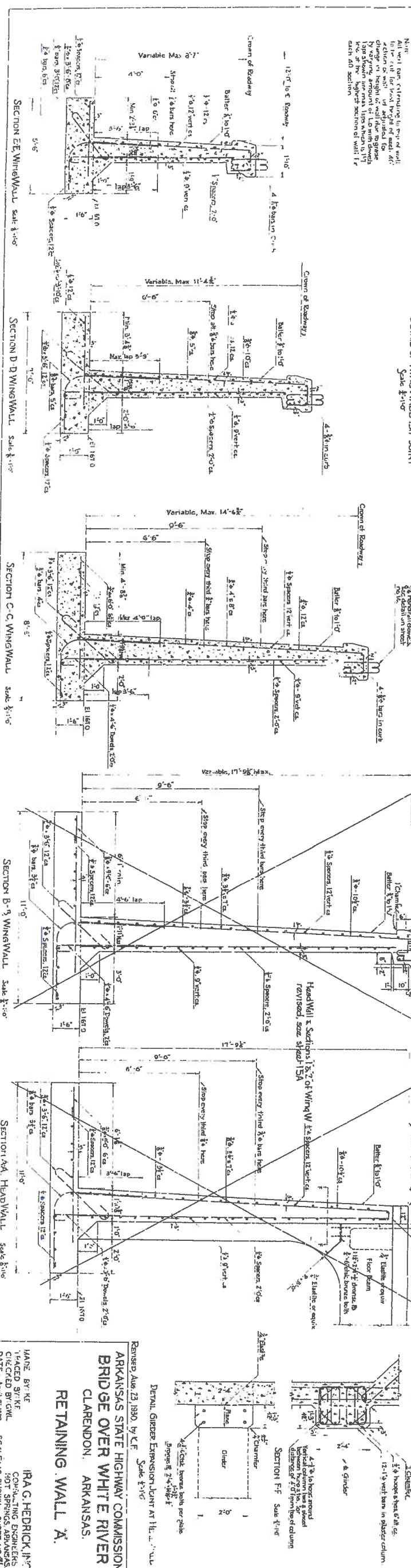


2nd Elastic of continuous strip
beant as shown

Note: Expansion joint for base of wall to be same as shown above except the portion perpendicular to wall extend to face of heel of base.

DETAIL OF WINGWALL EXP JOINT
Scale 1/4"=1'-0"

Note. All well runs, extending to top of wall to be cut for level by ft. of each. At a section of wall is adjusted for change in height of wall due to grade by varying amount of L.D. with diameter. L.D. shown are maximum which is 15 ft. at the highest section of wall for each 40 section.



DETAIL GIRDER EXPANSION JOINT AT FLOOR LEVEL

ARKANSAS STATE HIGHWAY COMMISSION
BRIDGE OVER WHITE RIVER
CLARENDON ARKANSAS.

RETAINING WALL "A"

MADE BY: KE
PLACED BY: KE
CHECKED BY: CWL
DATE: April 21/2010
SCALE: AS SHOWN
SHEET NO: 21

IRAC, G. HEDRICK, INC.
CONSULTING ENGINEERS
HOT SPRINGS, ARKANSAS

Dr. No. 1253
Drawing No. 0912



SECTION OF REMAINING WALL
B.C. DIST. South of 110°
RET WALL "B" → Be 01253
RET WALLS "C" & "D" → Be B1253
(RET WALLS E & F → Gone,
Removed w/ rest of A1253)

BR 105 01253 AND B1253

MADE BY ES.
TRACED BY ES.
CHECKED BY D.W.L.
DATE: APRIL 27, 1930. SCALE: AS SHOWN. SHEET NO. 23

IRAC, HEDRICK INC.
CONSULTING ENGINEERS
HOT SPRINGS, ARKANSAS.

DE-16. No 4914

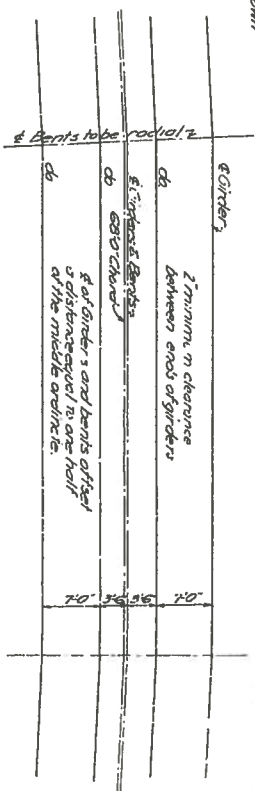
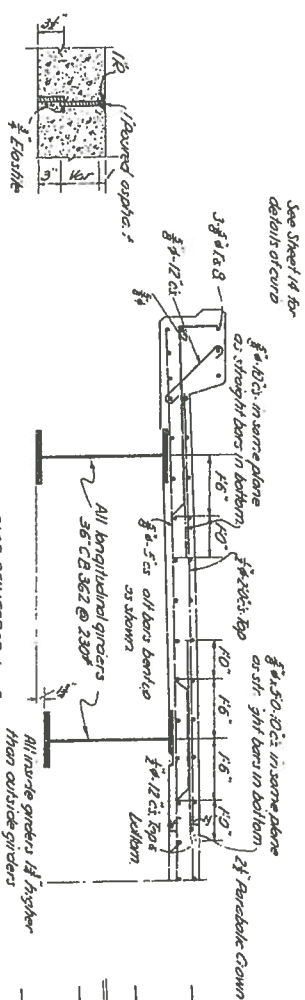
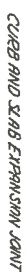


DIAGRAM SHOWING ARRANGEMENT OF
GIRLERS ON HORIZONTAL CURVES.

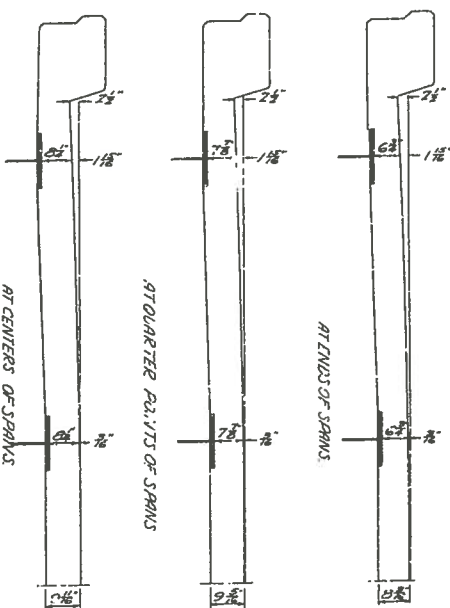
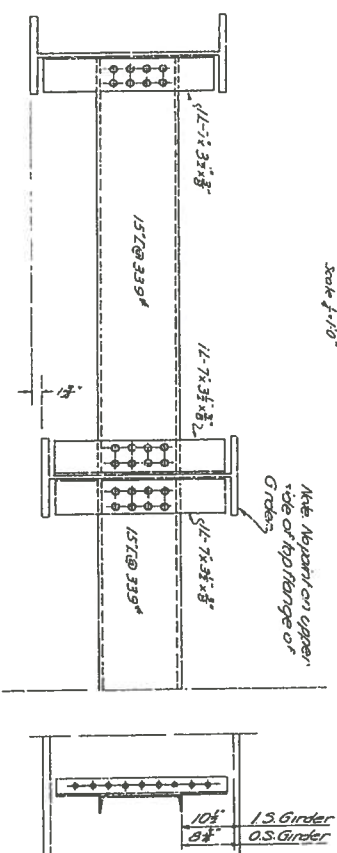
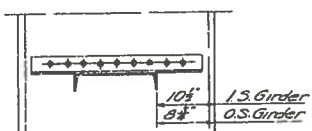


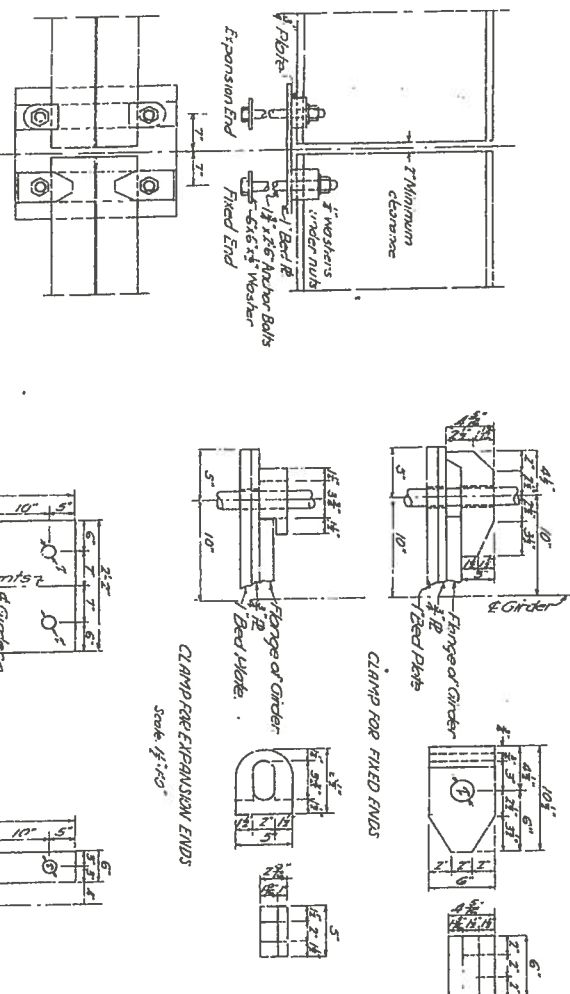
DIAGRAM SHOWING ARRANGEMENT
OF GIRDERS ON TANGENTS.



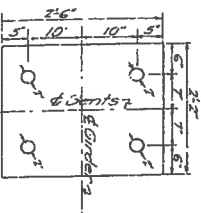
PLAN SHOWING TRANSVERSE BRACING



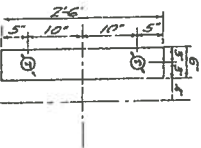
DETAILS OF BRACING



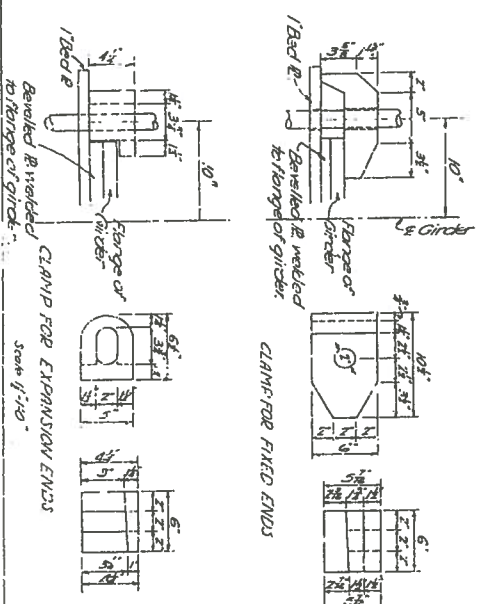
DETAILS FOR GIRDERS ON LEVEL GRADE



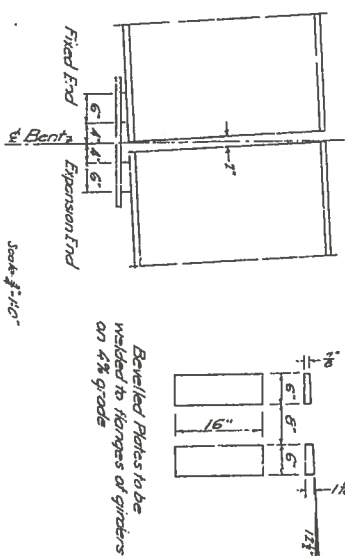
1" Bed Plates for rail Bents
except expansion joints or
Returning Walls & at Pier.



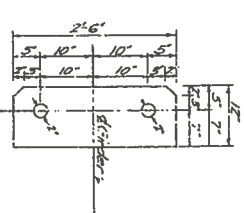
DETHLE $\sim \frac{3}{4}$ PLAYS
FOR GIBBLES & EVEL GRAD



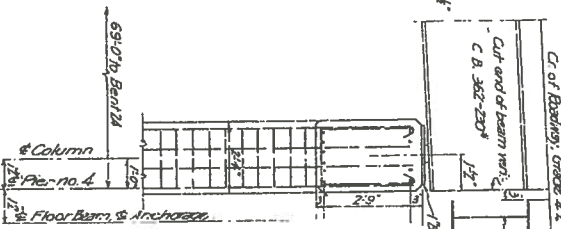
DETAILS FOR GIRDERS ON 4% GRADE.



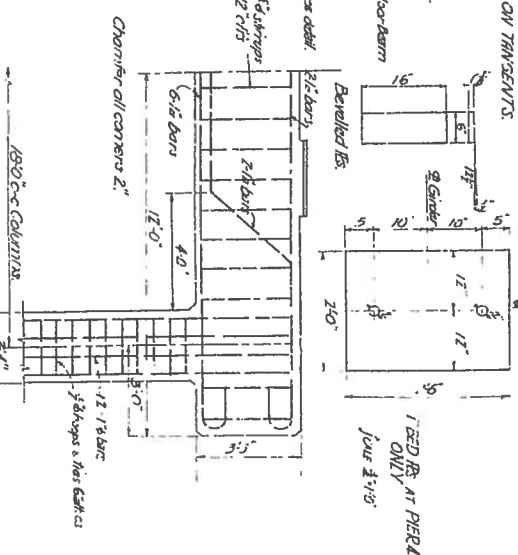
METHOD OF COMPENSATING FOR DEFLECTION OF GIRDER.



1" Bed Plates for expansion joints of Retaining Walls, as shown on Sheet No. 16.



DETAILS OF CAPS ON COLUMNS
AT ANCHORAGE PIER NO. 2



1200 RS AT PIERA
ONLY
JUNE 11:05

ARKANSAS STATE HIGHWAY COMMISSION
BRIDGE OVER WHITE RIVER
CLARENDON, ARKANSAS
DETAILS OF WEST APPROACH

MADE BY *LD* IRGA G. HEDRICK, INC.
TRACED BY *LD* CONSULTING ENGINEERS
CHECKED BY *ES* HOT SPRINGS, ARKANSAS
DATE *04/28/90* SCALE: *As shown* SHEET NO. *24*
DRWG. NO. 4915

BR/Na B1253)

